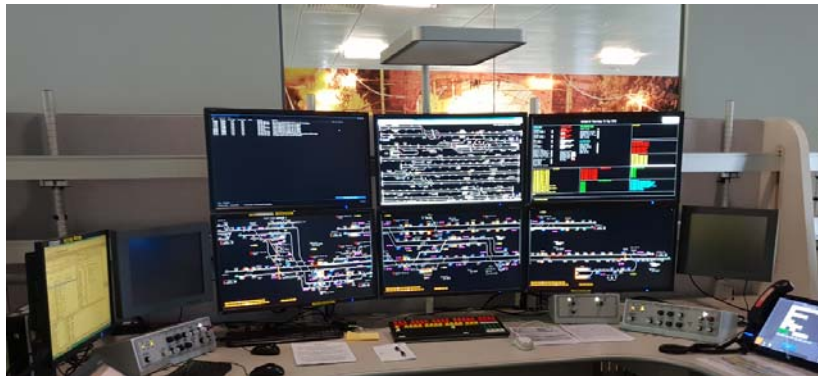


Transferable Lessons from Serious Operational Incidents / Investigations

**Issue No:** NR/OPS/037**Date Issued:** 31/08/2018

Train signalled towards a line under possession, Carstairs on WCML

Insert Picture / Diagram:**Summary of Incident:**

At 1840hrs the driver of 0Z03 reported that they have come to a stand at possession limit board and detonators, the Signaller signalled 0Z03 forward after being told by the signaller in the adjacent box that the possession had been given up.

Signaller at WSSC, Motherwell WS gave the PICOP permission to shorten the possession item 9 as booked, but when the signaller at Motherwell SC asked about the possession they were advised the possession was given up. Signaller at WSSC, Motherwell WS was working a newly commissioned work station with faults on equipment and a telephone system that was not working.

Learning points:

- Signaller on Motherwell WS although supported with an additional member of staff, should have requested that each critical task was dealt with separately and not to try and multitask especially when switching in a new workstation with the critical faults on the work station phone system.
- Key commissioning staff should remain well back from the work station during shift handover, to give the signaller time to handover and opportunity settle and gather information on the status of the work station and associated equipment.

Points to consider and further actions and precautions:

Early investigations into the incident have indicated that the signaller stepping onto a newly commissioned work station became overwhelmed by the number of faults and personal and the communications between the 2 signallers was misunderstood in that the signaller at Motherwell SC believed the possession had been removed and not shortened. Management of engineers and staff associated with commissioning to be kept back from the work station until the signaller requests their assistance or has settled on the work station.

Briefed out to:

Frontline Ops Staff and Operational Managers

NATIONAL OPERATING PROCEDURES

TRANSFERRABLE LESSONS FROM SERIOUS OPERATIONAL INCIDENTS / INVESTIGATIONS

Form: F3.27A

Issue: 01

Date: 02/09/17